

Maryland Historical Trust

Maryland Inventory of Historic Properties Number: CH 516

Name: Gov. Harry W. Nice, Mem. Rec. (#8039) US301

The bridge referenced herein was inventoried by the Maryland State Highway Administration as part of the Historic Bridge Inventory, and SHA provided the Trust with eligibility determinations in February 2001. The Trust accepted the Historic Bridge Inventory on April 3, 2001. The bridged received the following determination of eligibly.

MARYLAND HISTORICAL TRUST

Eligibility Recommended	X
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Eligibility Not Recommended

Criteria:	A	B	C	D	Considerations:	A	B	C	D	E	F	G	None
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Comments:

Reviewer, OPS: Anne E. Bruder Date: 3 April 2001

Reviewer, NR Program: Peter E. Kurtze Date: 3 April 2001

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Maryland Inventory of Historic Properties
Historic Bridge Inventory
Maryland State Highway Administration
Maryland Historical Trust

MHT No. CH-376Name and SHA No. Governor Harry W. Nice Memorial Bridge**Location:**Street/Road Name and Number: U.S. Route 301 over the Potomac RiverCity/Town: Newburg ☒ vicinityCounty: CharlesOwnership: ☐ State ☐ County ☐ Municipal ☒ Other (Maryland Toll Authority)This bridge projects over: ☐ Road ☐ Railway ☒ Water ☐ LandIs the bridge located within a designated district: ☐ yes ☒ no☐ NR listed district ☐ NR determined eligible district☐ locally designated ☐ other

Name of District _____

Bridge Type:☐ Timber Bridge☐ Beam Bridge ☐ Truss-Covered ☐ Trestle ☐ Timber-and-Concrete☐ Stone Arch☐ Metal Truss Bridge☐ Movable Bridge☐ Swing ☐ Bascule Single Leaf ☐ Bascule Multiple Leaf☐ Vertical Lift ☐ Retractable ☐ Pontoon☐ Metal Girder☐ Rolled Girder ☐ Rolled Girder Concrete Encased☐ Plate Girder ☐ Plate Girder Concrete Encased☐ Metal Suspension☐ Metal Arch☒ Metal Cantilever☐ Concrete☐ Concrete Arch ☐ Concrete Slab ☐ Concrete Beam ☐ Rigid Frame☐ Other Type Name _____

Description:**Describe Setting:**

The Governor Harry W. Nice Memorial Bridge carries U.S. Route 301 across the Potomac River, connecting Charles County, Maryland, and King George County, Virginia. To the southwest, in Dahlgren, is the U.S. Naval Surface Weapons Center. To the northeast, near Newburg, is a treatment plant. The bridge is 1.7 miles long and 135 feet above the water.

Describe Superstructure and Substructure:

(Discuss points identified in Context Addendum, Section C)

According to the original report prepared by Greiner in 1938, the structure is a high level bridge with a

"horizontal clearance of 700 feet and a vertical clearance of 135 feet over the main ship channel of the Potomac. The main span over the channel forms the highest point in the roadway with 3.75 per cent grade approaches. The main channel span and the two side spans flanking it, comprise a cantilever unit, the main span of which is 800 feet long and the side spans of which are anchor spans each 366 feet 8 inches long. The cantilever units forming the approaches to this central unit are made up of alternate spans and cantilever spans 437 feet 6 inches and 500 feet long, respectively.

The main section of the bridge is approached from the Virginia end by 63 spans of concrete pile bent and steel beam trestle construction 3873 feet long, and four plate girder spans 100 feet long, connecting trestle with the main cantilever section. On the Maryland side of the river, the main cantilever section of the bridge is approached by three 100 feet plate girder spans and two 250 feet simple truss spans connecting the filled approach with the main cantilever section."

Discuss major alterations:

The Nice Bridge had its bridge deck replaced during the 1980s, had ATAC installation, had fender system replacement, and had toll plaza modifications during the 1990s.

History:

When Built: *March 1939 to December 1940*

Why Built: *to provide access from Maryland to Virginia*

Who Built: *Harris Structural Steel Company/Merritt, Chapman and Scott*

Who Designed: *J.E. Greiner and Company*

Why Altered: *n/a*

Was this bridge built as part of an organized bridge building campaign: yes

The Nice Bridge was constructed as part of Maryland's Primary Bridge Program, initiated during the 1930s as a way to provide access to and from areas previously isolated from other parts of the state. As such, the bridge is potentially eligible under Criterion A for its role in encouraging inter- and intrastate transportation and commerce.

Surveyor Analysis:

This bridge may have NR significance for association with:

☒ A Events ☐ B Person

☒ C Engineering/Architectural Character

Was the bridge constructed in response to significant events in Maryland or local history?

This bridge was constructed as part of Maryland's Primary Bridge Program, which included a bridge across the Susquehanna River from Havre de Grace to Perryville, either a bridge or tunnel across the Baltimore Harbor, and a bridge across the Chesapeake Bay. This program was developed during the 1930s as a way to provide a continuous north-south highway that would connect Philadelphia to Richmond and bypass the major metropolitan areas of Washington, D.C., and Baltimore. With automobile use on the rise, Maryland was committed to providing accessible roadways and bridges to connect its isolated areas.

When the bridge was built and/or given a major alteration, did it have a significant impact on the growth and development of the area?

Before the opening of the Nice Bridge, only ferry service was available between Maryland and Virginia along the Potomac River below Washington. With the advent of the bridge, traffic patterns shifted so that a steady flow of travelers infiltrated the area. Easy access from Maryland to Virginia helped stimulate growth on both sides of the river, but more importantly, aided in boosting interstate travel as a whole.

Is the bridge located in an area which may be eligible for historic designation and would the bridge add to or detract from the historic and visual character of the possible district?

The Nice Bridge may be in an area that is eligible for historic designation and would add to both the historic and visual character of the possible district.

Is the bridge a significant example of its type?

This bridge is a well-preserved example of the metal cantilever bridge. Additionally, it is the only known example of a metal cantilever bridge in the state of Maryland.

Does the bridge retain integrity of the important elements described in the Context Addendum?

This bridge possesses integrity of location, design, setting, materials, workmanship, feeling and association. As mentioned above, very few significant alterations have occurred at this bridge. Thus, the bridge retains integrity of all of its original components, including the anchor arms, cantilever arms, central suspended span, piers, and abutments.

Is the bridge a significant example of the work of the manufacturer, designer, and/or engineer and why?

This is the only known example of a metal cantilever bridge in Maryland designed by Greiner. It is potentially eligible under Criterion C for its manufacture and design.

Should this bridge be given further study before significance analysis is made and why?

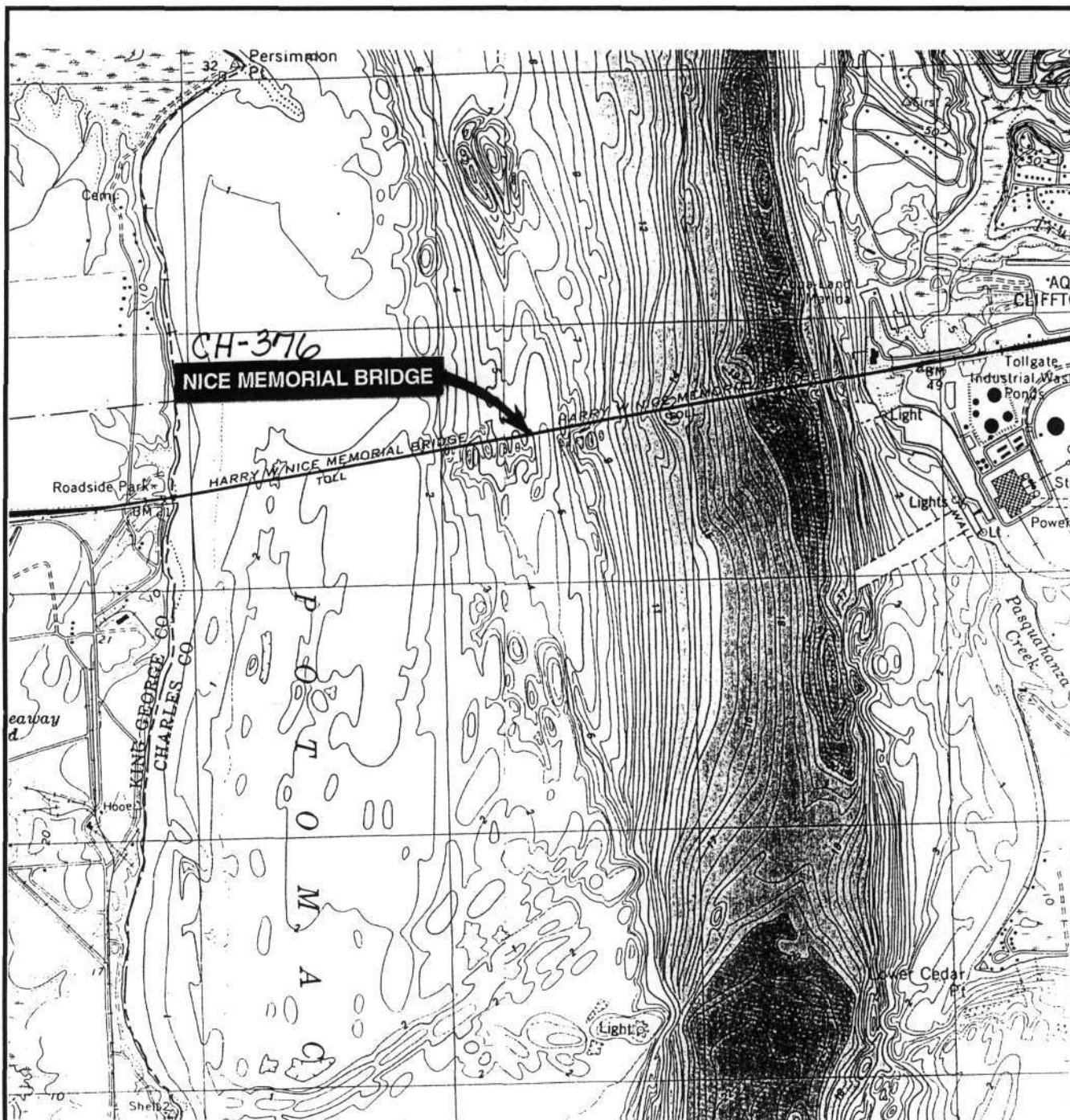
No further evaluation is necessary to determine National Register significance. However, additional research concerning the history of this bridge and its surroundings may be useful in providing a more complete picture of the bridge's background.

Provide black and white prints and negatives and color slides of bridge, details, and setting labeled according to NR Bulletin 16A and Maryland Supplement to Bulletin 16A.

Provide a photocopy USGS map illustrating the location of the bridge.

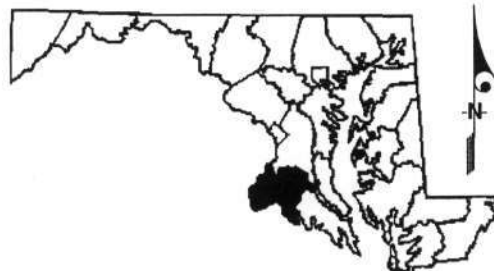
Surveyor:

Name:	<u>Alice Crampton/Julie Abell</u>	Date:	<u>12/16/94</u>
Organization:	<u>Parsons Engineering Science, Inc.</u>	Telephone:	<u>(703) 591-7575</u>
Address:	<u>10521 Rosehaven Street</u>		
	<u>Fairfax, Virginia 22030-2899</u>		

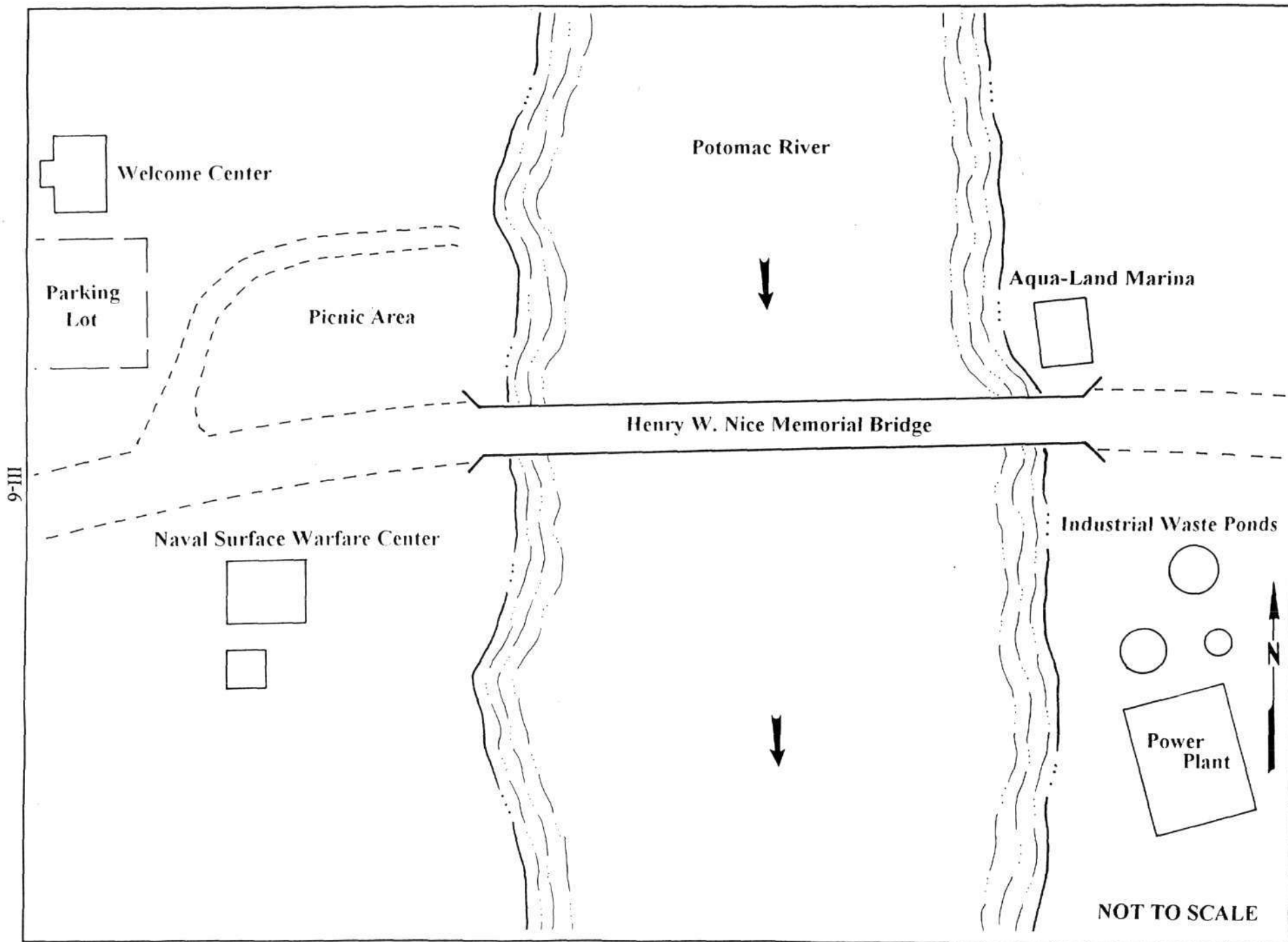


Charles County

US 301 over Potomac River - Governor Harry W. Nice Memorial Bridge



Scale 0 1000 2000 feet
0 0.5 kilometer



CH-376



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland

Julie Abell

12/94

Maryland State Highway Administration
Northwest elevation

1 of 9



SH-376
Governor Harry W. Nice Memorial Bridge
Charles County, Maryland
Julie Abell

12/94

Maryland State Highway Administration
Northwest elevation, detail

2 of 9



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland

Julie Abell

12/94

Maryland State Highway Administration
Northwest elevation, detail

3 of 9

MANUFACTURED
BY KODAK



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland

Julie Abell

12/94

Maryland State Highway Administration
Southeast elevation

4 of 9



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland

Julie Abell

12/94

Maryland State Highway Administration
Southeast elevation, detail

5 of 9



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland
Julie Abell

12/94

Maryland State Highway Administration
Southeast elevation, detail

6 of 9



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland
Julie Abell

12/94

Maryland State Highway Administration
Southeast elevation, detail

7 of 9



HARRY W. NICKERSON
MEMORIAL BRIDGE

652



ROSELAND

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland
Julie Abell

12/94

Maryland State Highway Administration
Approach looking northeast
8 of 9



CH-376

Governor Harry W. Nice Memorial Bridge
Charles County, Maryland

Julie Abell

12/94

Maryland State Highway Administration
Approach looking southwest

9 of 9

MARYLAND HISTORICAL TRUST ADDENDUM SHEET Property Name: Governor Harry W. Nice Memorial Bridge
U.S. 301 SOUTH CORRIDOR TRANSPORTATION STUDY Survey No.: CH-376

Property Address U.S. 301 over the Potomac River, Newburg vicinity, Charles County
Owner Name/Address Maryland Toll Authority, P.O. Box 8, Newburg MD 20664
Year Built 1939-1940

Description:

The Governor Harry W. Nice Memorial Bridge was previously surveyed by Parsons Engineering Science, Inc. in 1994. Since the time of the previous survey, the property is unchanged.

National Register Evaluation:

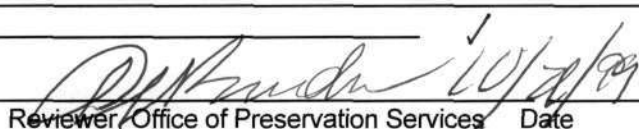
The Governor Harry W. Nice Memorial Bridge was determined eligible for the National Register of Historic Places by the Interagency Review Committee in June of 1996.

MHT CONCURRENCE:

Eligibility ☒ recommended ☐ not recommended

Criteria ☐ A ☐ B ☒ C ☐ D Considerations ☐ A ☐ B ☐ C ☐ D ☐ E ☐ F ☐ G ☐ None

Comments: _____

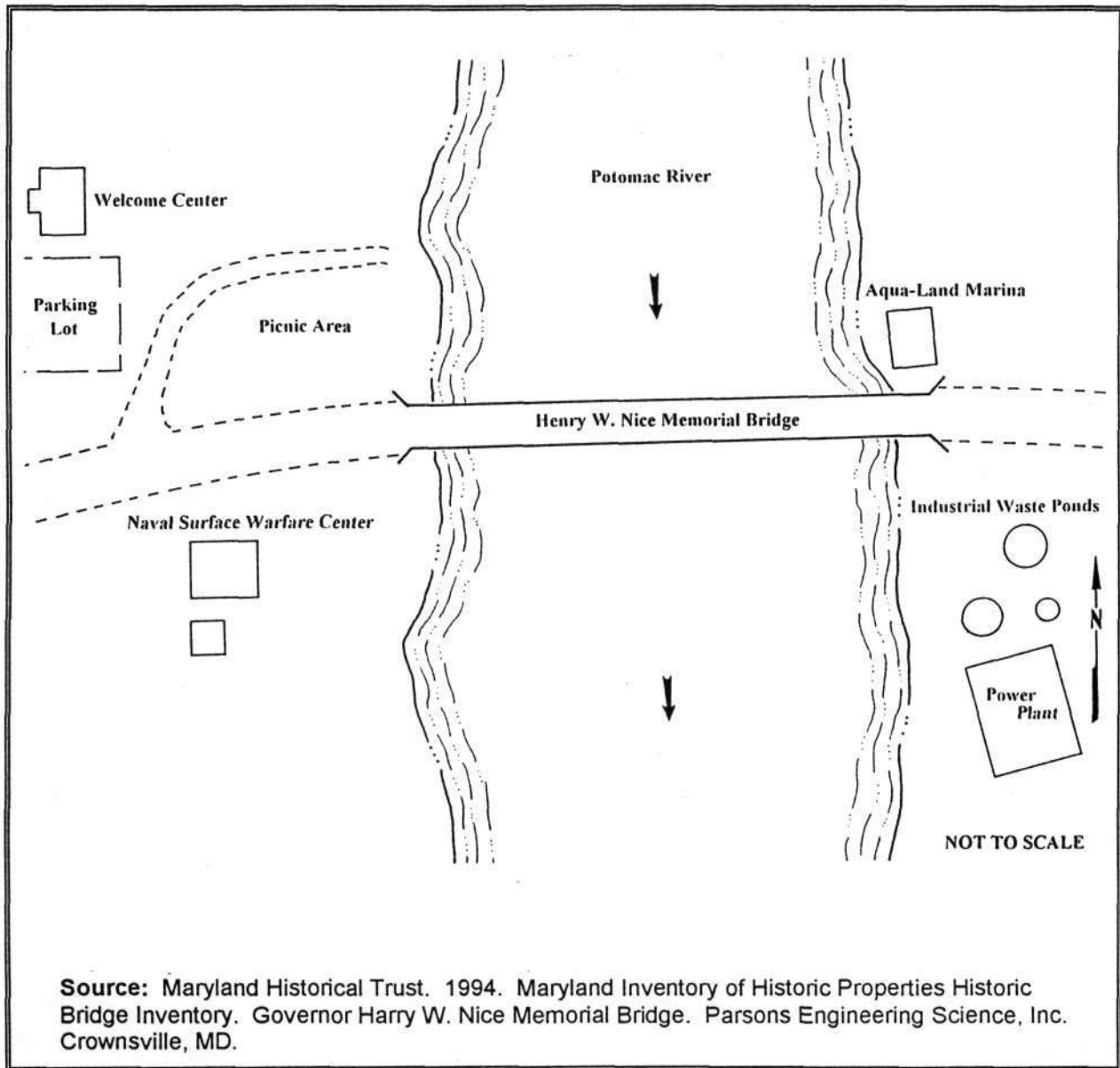
 10/2/98
Reviewer, Office of Preservation Services Date

Reviewer, NR program

Date

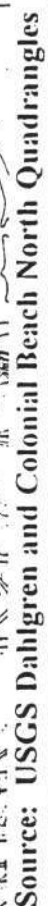
Property Address U.S. 301 over the Potomac River, Newburg vicinity, Charles County
Owner Name/Address Maryland Toll Authority, P.O. Box 8, Newburg MD 20664
Year Built 1939-1940

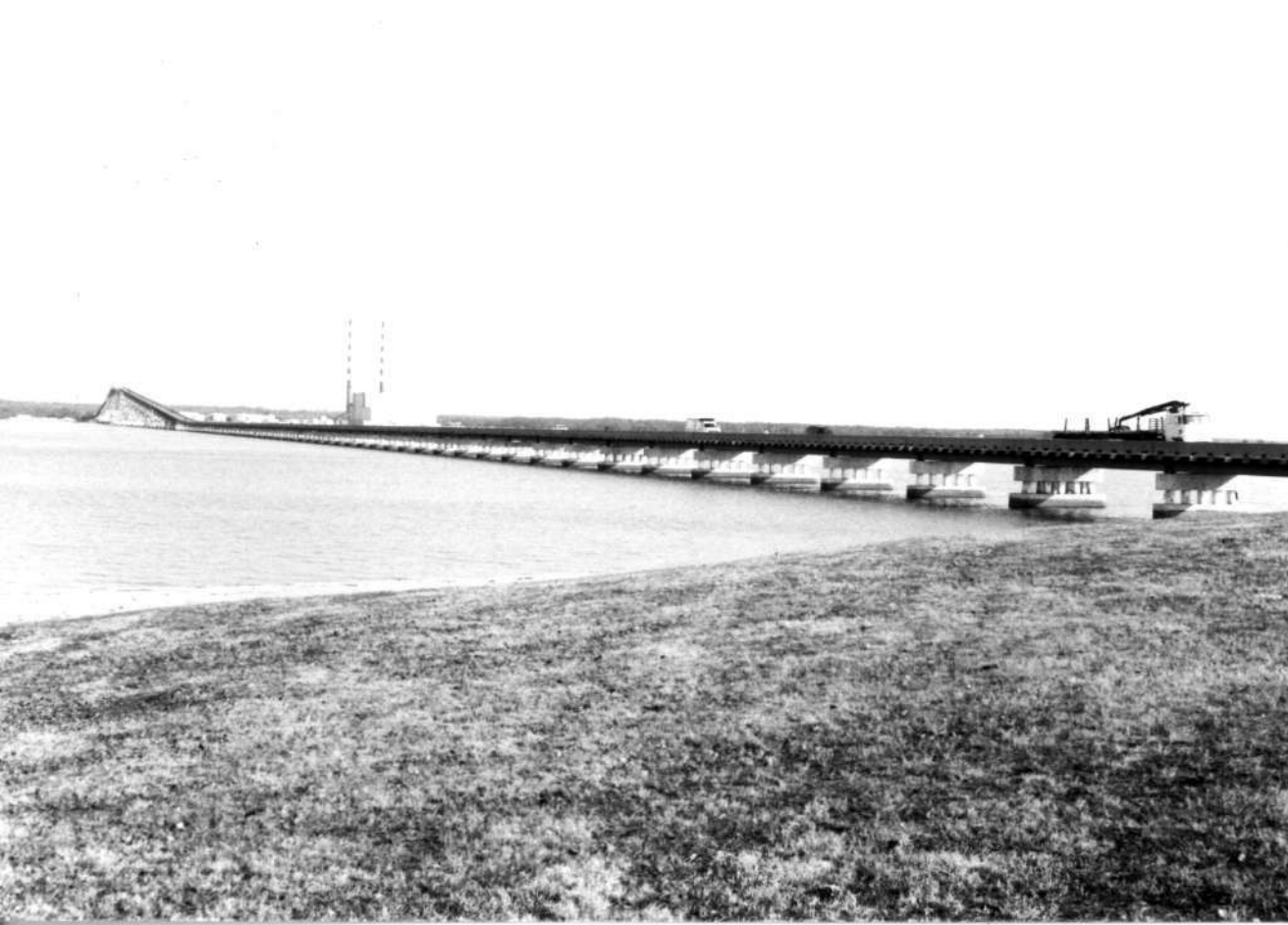
Resource Sketch Map:



Quadrangle COLONIAL BEACH N., MD/DALLIGREEN VA

Quadrangle COLONIAL BEACH N., MD/DALLIGREEN VA





1) CH- 376

2) Governor Harry W. Nice Memorial Bridge,
US 301 over Potomac River

3) Charles Co, MD and King George Co., VA

4) Tim Tamburrino

5) 1/99

6) MD SHPO

7) N elevation - View SE

8) 1 of 4



- 1) CH-376
- 2) Governor Harry W. Nice Memorial Bridge,
US 301 over Potomac River
- 3) Charles Co, MD and King George Co, VA
- 4) Tim Tamburrino
- 5) 1/99
- 6) MD SHPO
- 7) N elevation - View SE
- 8) 2 of 4



1) CH-376

2) Governor Harry W. Nice Memorial Bridge,
US 301 over Potomac River

3) Charles Co, MD and King George Co, VA

4) Tim Tamburrino

5) 1/99

6) MD SHPO

7) S elevation of bridge

8) 3 of 4



1) CH-376

2) Governor Harry W. Nice Memorial Bridge
US 301 over Potomac River

3) Charles Co, MD and King George Co, VA

4) Tim Tamburrino

5) 1/99

6) MD SHPO

7) Operations Building - View N

8) 4 of 4

NR Eligible: yes X
no

Date _____

200801408

A one-story, one-room-deep, contemporary center block occupies the central portion of the north elevation of the front block. A single-leaf, one-light, steel door is located to the west of the center block on the north elevation of the front block. A set of paired, replacement, four-light, awning-sash, aluminum windows occupies the west elevation of the center block. The east elevation is devoid of openings. The east and west elevations of the front block feature a one-story, one-bay, circa-1960, concrete-block, flat-roof addition whose walls are shorter than the front block. The additions feature single sets of paired, four-light, replacement, awning-sash, aluminum windows. The west elevation features a single-leaf, steel door.

A one-story, two-room-deep, contemporary addition conceals the north elevation of the one-room-deep addition. Two replacement, aluminum, multi-light, casement windows occupy the east elevation of the addition. The addition features a replacement, aluminum, garage door on the west elevation. A single-leaf, two-light, steel door is situated to the south of the garage door. The only bay visible in the north elevation of the two-room addition is a set of paired, replacement, aluminum windows. The easternmost window is covered by a louvered, aluminum vent.

A one-story, one-bay, circa-1960, frame, gable-roof addition is centrally located on the north elevation of the two-room, 1940 block. The addition is clad in horizontal composite board. The east elevation features two window-like openings. Wooden boards cover the southernmost bay and an air conditioning unit is located in the northernmost bay. The west elevation features one centrally located, single-leaf, three-light, steel door.

A large, circa-1983, brick addition is attached to the north end of the frame addition. The addition contains an aluminum, replacement, two-light, overhead garage door in the north elevation. A single-leaf, steel door is located in the west elevation. Three-light, replacement, awning-sash, aluminum windows are located in the west and east elevations.

The interior of the building has been largely altered in order to accommodate the continued use of the building.

Overall, the building, which in 2008 houses the maintenance facility for the bridge, is in good condition.

Landscape Features:

A grass lawn, interspersed with mature evergreen and deciduous trees, surrounds the building complex. Mature evergreen trees screen the front block. A chain-link fence extends from the east and west elevations of the building and encloses the northern portion of the property, which includes a paved-asphalt parking lot and a poured-concrete maintenance area. Two poured-concrete, in-ground septic tanks are located to the east of the rear, circa-1983 addition. A circa-1983, corrugated metal garage occupies the northwest portion of the property.

Historical Narrative:

The Potomac River Bridge Administration Building was erected in 1940 to house the administration, maintenance, and police functions for the Potomac River Bridge (as of 1968, the Governor Harry W. Nice Memorial Bridge). The Governor Harry W. Nice Memorial Bridge (Nice Bridge) is the second oldest of the Maryland Transportation Authority's (Authority) seven toll facilities.

The Nice Bridge carries U.S. 301 (Crain Highway) over the Potomac River connecting Charles County, Maryland and King George County, Virginia. The 1.7-mile bridge, designed by J.E. Greiner and Company, was erected from September 1938 to December 1940 by the Harris Structural Steel Company and Merritt, Chapman, and Scott. The bridge was constructed as part of Maryland's Primary Bridge Program initiated during the 1930s and approved by Maryland Governor Harry W. Nice as a way to

MARYLAND HISTORICAL TRUST REVIEW

Eligibility recommended _____

Eligibility not recommended _____

Criteria: ___ A ___ B ___ C ___ D Considerations: ___ A ___ B ___ C ___ D ___ E ___ F ___ G

MHT Comments:

Reviewer, Office of Preservation Services

Date

Reviewer, National Register Program

Date

provide access to and from areas previously isolated from other parts of the state. Before the opening of what would become the Nice Bridge, only ferry service was available along the Potomac River between Maryland and Virginia. Those traveling via roadways from Baltimore to Richmond had to use the sluggish US 1 through Washington, D.C. (Crampton and Abell 1994).

In September 1938, President Franklin D. Roosevelt traveled to Charles County, Maryland to attend the ground-breaking ceremony for the new bridge. To serve the bridge, Maryland Route 3 (Crain Highway) was extended from Mattawoman in northern Charles County to the Potomac River. This span of roadway would eventually become US 301.

On December 14, 1940, Governor Herbert R. O'Connor and the State Roads Commission of Maryland held the opening ceremonies for the Potomac River Bridge in the Administration Building (Maryland Transportation Authority). More than 1,000 people attended the dedication ceremony. Following the dedicatory speech by Governor O'Connor, Mrs. O'Connor cut the ceremonial black and gold ribbon admitting the first official traffic to the bridge (Baltimore American, Sunday, December 15, 1940, 14-L). Motorists could cross toll-free until 9 PM that evening and hundreds of cars took advantage (Ibid.).

With the rising use of automobiles throughout the nation, the construction of the bridge shifted traffic patterns so that a steady flow of automobiles traveled to the area. Easy access from Maryland to Virginia helped to stimulate growth on both sides of the Potomac River and aided in boosting interstate travel as a whole (Crampton and Abell 1994). On Monday, April 15, 1968, the State Roads Commission of Maryland renamed the former Potomac River Bridge the Governor Harry W. Nice Memorial Bridge after former Maryland Governor Harry W. Nice who served Maryland from 1935 until 1939.

The Maryland State Roads Commission (the predecessor to the Authority) erected the Potomac River Bridge Administration Building in 1940 to house the various administration, maintenance, security, toll collections, and ticket office for the bridge. The T-shaped building continued to house these functions until June 8, 1983, when a new administration building was erected to the east of the 1940 building on the site of an old Sunoco Gas Station. In 1983, the original toll plaza was razed and a new one built in its stead. Since 1983, the former Potomac River Bridge Administration Building has housed the maintenance facilities for the bridge (Swann 2007). Presumably, the Authority constructed the circa-1983 addition and the metal garage in order to house additional maintenance functions and supplies.

Significance Evaluation:

The Potomac River Bridge Administration Building is eligible under Criterion A as a contributing resource to the National Register-eligible Nice Bridge. The Nice Bridge was determined eligible in 2001 under Criterion A for its role in encouraging interstate and intrastate transportation and commerce as part of Maryland's Primary Bridge Program and under Criterion C as the only known example of a metal cantilever bridge in Maryland designed by J.E. Greiner and Company. The Administration Building was not evaluated as part of the 2001 study. The Potomac River Bridge Administration Building was erected in 1940 to house all of the administration, security, maintenance, and toll facilities for the bridge, and therefore, supports the bridge's eligibility under Criterion A.

According to the National Register of Historic Places guidelines established by the National Park Service, the quality of significance in American history, architecture, archeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association, and:

- A. that are associated with events that have made a significant contribution to the broad patterns of our history;
- B. that are associated with the lives of significant persons in our past;
- C. that embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or

MARYLAND HISTORICAL TRUST REVIEW

Eligibility recommended _____

Eligibility not recommended _____

Criteria: ___ A ___ B ___ C ___ D Considerations: ___ A ___ B ___ C ___ D ___ E ___ F ___ G

MHT Comments:

Reviewer, Office of Preservation Services

Date

Reviewer, National Register Program

Date

that possess high artistic values, or that represent a significant and distinguishable entity whose components lack individual distinction; or
D. that have yielded or may be likely to yield information important in history or prehistory (National Park Service 1997).

The Potomac River Bridge Administration Building is eligible under Criterion A as a contributing resource to the Governor Harry W. Nice Memorial Bridge (formerly the Potomac River Bridge). The bridge and Administration Building were completed in 1940 as part of Maryland's Primary Bridge Program created by the State Roads Commission to encourage interstate and intrastate transportation and commerce. The Administration Building was erected to support the administrative, security, maintenance, and financial functions of the bridge.

The Potomac River Bridge Administration Building is not eligible under Criterion B as it has no known association with the lives of individuals of historical importance.

The Potomac River Bridge Administration Building is not eligible under Criterion C as it does not possess the architectural distinctiveness necessary to qualify it for listing in the National Register of Historic Places. The building does not represent a type, period, or method of construction nor does it possess high artistic values or represent the work of a master.

The Potomac River Bridge Administration Building was not evaluated under Criterion D as part of the architectural survey and evaluation.

The Potomac River Bridge Administration Building retains integrity from the period of significance (1940-1958). The original footprint of the building, despite the post-1958 additions to the north (rear) of the building and the small, post-1958 wings to the east and west, is still discernible; therefore, the building retains integrity of design. The building retains its original location on the north side of U.S. 301 adjacent to the bridge; however, the setting surrounding the building has changed as the building is surrounded by post-1958 commercial, residential, and industrial development; therefore, the building retains integrity of location but does not retain integrity of setting. The building contains replacement windows, but retains the original exterior materials, doors, and openings; therefore, the building retains integrity of materials and workmanship. The retention of the entry and the Maryland State Seal enhance integrity of materials and workmanship. The combination of integrity of design, location, materials, and workmanship culminates in the building's ability to convey its feeling and association as a 1940 administration building that housed the ancillary functions for the Governor Harry W. Nice Memorial Bridge.

Boundary Description and Justification:

The proposed National Register Boundary for the building includes a 0.25-acre lot centered on the building. The building is bounded by US 301 to the south, a grove of trees to the west, an unnamed ancillary road to the north, and a paved-asphalt driveway to the west. The boundary includes the footprint of the building and the immediate grounds historically associated with the building. The boundary excludes the modern maintenance shed and garages located to the northwest of the building and the 1983 Administration Building to the east.

References

Crampton, Alice and Julie Abell

1994 Governor Harry W. Nice Memorial Bridge (#8039), CH-376, Determination of Eligibility Form. Prepared by Parsons Engineering Science, Inc. for the Maryland State Highway Administration. On file at the Maryland Historical Trust: Crownsville, Maryland.

MARYLAND HISTORICAL TRUST REVIEW

Eligibility recommended _____ Eligibility not recommended _____

Criteria: ___ A ___ B ___ C ___ D Considerations: ___ A ___ B ___ C ___ D ___ E ___ F ___ G

MHT Comments:

Reviewer, Office of Preservation Services

Date

Reviewer, National Register Program

Date

Maryland Transportation Authority

Vertical File, Governor Harry W. Nice Memorial Bridge, located at the Governor Harry W. Nice Memorial Bridge Administration Building, Morgantown, Maryland.

National Park Service

1997 National Register Bulletin: How to Apply the National Register Criteria for Evaluation, U.S. Department of the Interior: Washington, D.C.

Swann, Arlene

2007 Personal Interview with Barbara Frederick, 14 December. Notes on file at A.D. Marble & Company, Owings Mills, Maryland.

MARYLAND HISTORICAL TRUST REVIEW

Eligibility recommended _____

Eligibility not recommended _____

Criteria: ____ A ____ B ____ C ____ D Considerations: ____ A ____ B ____ C ____ D ____ E ____ F ____ G

MHT Comments:

Reviewer, Office of Preservation Services_____
Date_____
Reviewer, National Register Program_____
Date

Potomac River Bridge (Nice Bridge) Administration Building, CH-376

Crain Highway, Morgantown, Maryland



Map Document: projects\p43\mapping\MD_sals\CH-376.mxd
1/16/2008 -- 8:11



2,000 0 2,000
Feet

Resource

Map Source: USGS 7.5' DRGs: Colonial Beach North, MD;
Dahlgren, VA; Mathias Point, MD; Popes Creek, MD.



CH-376

POTOMAC RIVER BRIDGE (NICE BRIDGE) Administration Building
Charles County, MARYLAND

B. FREDERICK

12.2007

MD SHPO

SOUTH elevation; view TO NORTHEAST

Photo # 1 of 9



CH-376

POTOMAC RIVER BRIDGE (NICE BRIDGE) Administration Building

Charles County, MARYLAND

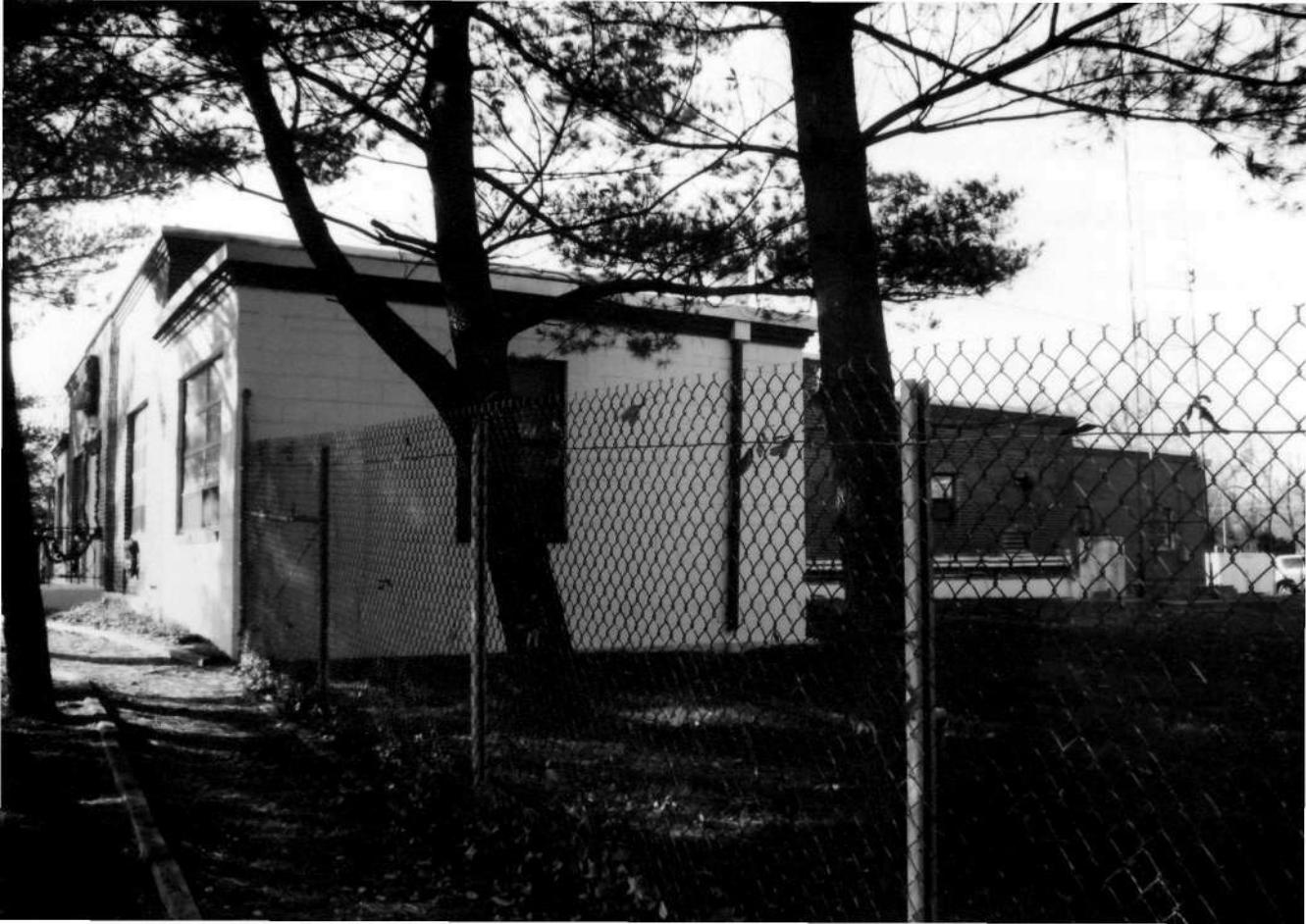
B. Frederick

12.2007

MD SHPO

SOUTH & WEST elevations; view to northeast

Photo # 2 of 9



CH-376

POTOMAC RIVER BRIDGE (WICE BRIDGE) ADMINISTRATION BUILDING
CHARLES COUNTY, MARYLAND

B. FREDENICK

12.2007

MD SHPO

SOUTH & EAST ELEVATIONS; VIEW TO NORTHWEST

Photo # 3 of 1



CH-376

POTOMAC RIVER BRIDGE (NICE BRIDGE) ADMINISTRATION BUILDING

CHARLES COUNTY, MARYLAND

B. FREDENICK

12.2007

MD SHPO

WEST ELEVATION; VIEW TO SOUTHEAST

PHOTO # 4 OF 9



CH-376

Potomac River Bridge (Nice Bridge) Administration Building

Charles County, MARYLAND

B. Frederick

12.2007

MP SHPO

west elevation; view to east

Photo # 5 of 9



CH-376

Potomac River Bridge (Nice Bridge) Administration Building
Charles County, MARYLAND

B. Fredenick

12.2007

MD SHPO

North is ~~west~~ elevations; view to southeast

Photo # 6 of 9



CH-376

POTOMAC River Bridge (Nice Bridge) Administration Building
Charles County, MARYLAND

B. Frederick

12.2007

MD SHPO

North is east elevations; view to southwest

Photo # 7 of 9



FLAMMABLE
NO SMOKING

HAZARD
CLASSIFICATION
GHS 02

HAZARD CLASSIFICATION
GHS 02

CH-376

Potomac River Bridge (Nice Bridge) Administration Building
Charles County, MARYLAND
B, Frederick

12.2007

MD SHPO

North elevation; view to southwest

Photo # 8 of 9



CH-376

Potomac River Bridge (Wice Bridge) Administration Building
Charles County, MARYLAND

B. Fredenick

12.2007

MD STIPD

South & west elevations; view to east towards 1983 toll plaza

Photo # 9 of 9

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